

Summary Design Report

Downtown & Waterfront Enhancements

Catskill Creek Trail Loop
Bridge Street Enhancements
Water Street Enhancements
Main Street Enhancements

Prepared by:
Greene County Economic Development,
Tourism & Planning
Village of Catskill, New York

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Submitted by:



Planning / Design / Landscape Architecture PLLC

Saratoga Springs, NY

Funding:



Catskill Downtown and Waterfront Enhancements Summary Design Report

Project Advisory Committee

Vincent Seeley, President, Village of Catskill
Joe Leggio, Supervisor, Town of Catskill
Pat Walsh, Catskill Town Board
Peter Markou, Greene County Treasurer
Joseph D. Kozloski, Catskill Village Board
Brian Kehoe, Catskill Village Board
Linda Overbaugh, Catskill Chamber
Bill Zwoboda, Planning Board Chair, Village of Catskill
Lew O'Conner, Superintendent, Catskill Highway Department

Greene County Legislators Serving Catskill

Vincent Seeley, District 1
Linda Overbaugh, District 1
Joseph D. Kozloski, District 1
Kevin Lennon, District 1

(Former Greene County Legislators serving on Project Advisory Committee: Forest Cotton, Karen Deyo, Joe Izzo, Keith Valentine)

Project Partners

Rene VanSchaack, Executive Director, Greene County IDA
Jeff Flack, Executive Director, Greene County Soil and Water Conservation District
Gary Harvey, Superintendent, Greene County Highway
Stan Wase, Engineering Division, Greene County Highway

Department of State Representative

Bonnie Devine

Coordinated by:

Greene County Economic Development, Tourism and Planning Department
Warren Hart, Director
Ed Diamante, Principal Planner

Consultants:

Elan Planning/Design/Landscape Architecture PLLC

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Catskill Downtown and Waterfront Enhancements Draft Summary

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Introduction

The Village of Catskill located in Greene County, is a Hudson River community rich in history and scenic beauty. Through sequential planning efforts that build upon the previous effort, residents are striving to maintain and revitalize downtown by capitalizing on a major waterfront asset, the Catskill Creek. Catskill Creek is a Hudson River tributary shouldered on both sides by the Village's Central Business District. This waterside setting has over time presented both challenges and opportunities to downtown's vitality. Today, Catskill is dedicating energy and resources to restore this waterfront into an engine for tourism and economic development and a conduit to surrounding community life.

In 2008, the Village of Catskill prepared a *Local Waterfront Revitalization Plan* and a *Downtown and Waterfront Revitalization Strategy* which identified a number of catalytic projects designed to revitalize the Village's downtown by capitalizing on the community's greatest natural resource, the Catskill Creek.

As a result of completing these studies, Greene County was awarded grant funding from the NYSDOS Division of Coastal Resources to further develop planning and design efforts focused on both the waterfront districts and the links to Main Street. As part of the grant, individual projects have been identified within the study area. These project areas are targeted for enhancement recommendations to best meet grant goals and objectives.

This downtown waterfront enhancements study is addressed as four distinct project areas identified as the:

- Catskill Creek Trail Loop
- Bridge Street Streetscape
- Water Street Streetscape
- Main Street Streetscape

The project areas have been advanced to various design levels based on different project objectives for the specific area. The overarching goal of this project is to provide improved access to and around the Catskill Creek waterfront and to strengthen connections to Downtown and Main Street, which is located two blocks from the waterfront. Improved pedestrian connections have been addressed in areas adjacent to the waterfront and Main Street that include commercial properties, a public school, and residential neighborhoods. The suggested improvements address access, safety, aesthetics, and walkability.

While already having substantial boating activity, the Catskill Creek has the potential to provide enhanced opportunities for recreation and businesses benefitting from the water setting. Implementation of the suggested streetscape and trail improvements will bolster the ongoing efforts of the local and state governments to attract creative and strategic development of the downtown to have its potential realized. When these projects are implemented, it will also demonstrate the fulfillment of a significant number of the LWRP's goals and objectives.

Existing Conditions

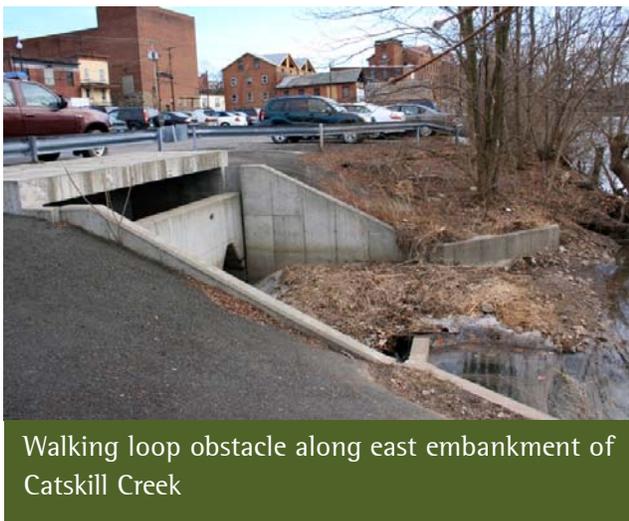
Several site visits to evaluate and photo document existing project area conditions were completed. These evaluations and observations, coupled with input from the Project Advisory Committee, identified the positive and negative attributes of the sites. The site evaluations incorporated the proposed functional use programming as well as the aesthetic preferences provided by the Project Advisory Committee.

Catskill Creek Trail Loop

Existing infrastructure and waterfront conditions were examined along both sides of the Catskill Creek from the Black Bridge to the north to the Uncle Sam Bridge to the south. The current uses along the waterfront includes school activities, paths, open green space, fishing, small craft boating, commercial (boating related), and limited industrial. Site investigation findings are listed below. Photos of existing conditions along the proposed trail loop are provided in *Drawing1.1*.

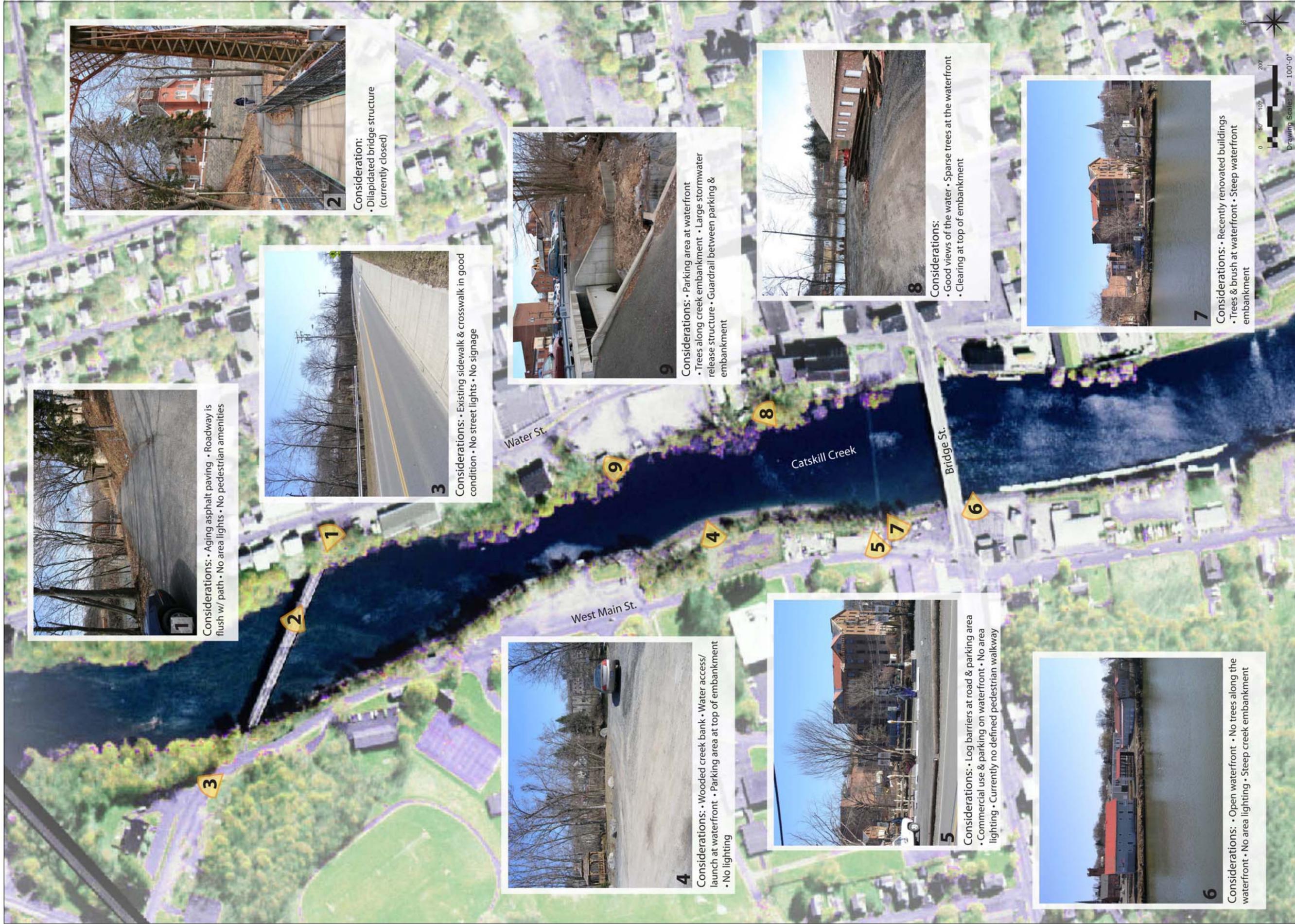
Existing Assets:

- Potential to utilize existing infrastructure
- Positive views and vistas along the water corridor
- Existing boat launch
- Adjacent proximity to Downtown
- Commercial use opportunities along the waterfront.



Areas for Improvement:

- Components of the existing infrastructure require renovation
- Areas of pedestrian and vehicular conflict require resolution for improved safety
- The County parking lot is a barrier to trail routing along the water's edge
- Erosion is problematic on the creek bank
- The Black Bridge has been closed for reasons of safety
- A large storm water release structure is an obstacle at the County parking area



1
 Considerations: • Aging asphalt paving • Roadway is flush w/ path • No area lights • No pedestrian amenities



2
 Consideration:
 • Dilapidated bridge structure (currently closed)



3
 Considerations: • Existing sidewalk & crosswalk in good condition • No street lights • No signage



4
 Considerations: • Wooded creek bank • Water access/launch at waterfront • Parking area at top of embankment • No lighting



9
 Considerations: • Parking area at waterfront • Trees along creek embankment • Large stormwater release structure • Guardrail between parking & embankment



5
 Considerations: • Log barriers at road & parking area • Commercial use & parking on waterfront • No area lighting • Currently no defined pedestrian walkway



8
 Considerations:
 • Good views of the water • Sparse trees at the waterfront • Clearing at top of embankment



6
 Considerations: • Open waterfront • No trees along the waterfront • No area lighting • Steep creek embankment



7
 Considerations: • Recently renovated buildings • Trees & brush at waterfront • Steep waterfront embankment



Lead Consultant:
ELAN
 PLANNING / DESIGN /
 LANDSCAPE
 ARCHITECTURE, PLLC
 118 DIVISION STREET
 STUDIO 304
 SARATOGA SPRINGS
 NEW YORK 12866

Project Sponsors:

 Contract No.: T007022

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Project Title:
Downtown & Waterfront Enhancements
 Village of Catskill
 Greene County
 New York

Sheet Title:
Trail Loop Existing Conditions Photo Assessment

DATE: 11.16.12
 DRAWN BY: ZB/RAB
 CHECKED BY: JET
 FILE: 08.008.2_Base
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DRAWING NO.:
1.1

Bridge Street

The portion of Bridge Street slated for enhancements encompasses both sides of the roadway from the Uncle Sam Bridge to its intersection with Main Street. The street corridor use is primarily commercial, retail, and office use. The physical condition of the roadway and sidewalk paving, while still useable, is in need of replacement. The character of this street will benefit greatly from streetscape improvements inclusive of street trees and lighting. Site investigation observations are listed below. Photos of existing conditions along Bridge Street are provided in *Drawing 1.2*.



Bridge Street

Existing Assets:

- Direct connection to downtown and to the waterfront
- Views of the water and bridge
- Proximity to vacant public and private lands that previous studies have identified for development
- Existing infrastructure is in place
- Sidewalks carried across driveways

Areas for Improvement:

- Ageing existing sidewalk and curbs
- Sidewalks are narrow in locations adjacent to buildings
- Worn crosswalks and lack of crosswalks
- Absence of street lighting
- No green strips, splash zones or tree plantings
- Large gaps along streetscape without building facades



Main Street and Bridge Street intersection



3

Considerations:

- Aging sidewalk/ curb (10' wide)
- No street lights & trees
- No adjacent building facades
- No green strip/splash zone



4

Considerations:

- Aging concrete sidewalk & curb
- Worn crosswalk striping
- Building entrances protrude into sidewalk
- No crosswalk on Bridge St. west of Water St.



1

Considerations:

- Worn crosswalk striping
- One area light
- Imposing utility poles & wires
- Underutilized green space
- Lacking pedestrian amenities



2

Considerations:

- Narrow sidewalk
- Expansive flush curb
- Aging concrete sidewalks & curb
- Sidewalks carried across driveway openings
- Multiple curb cuts



5

Considerations:

- Worn crosswalk striping
- Narrow sidewalk
- Aging concrete sidewalk & curb
- Sidewalk flush with adjacent parking



7

Considerations:

- Narrow sidewalk
- Aging concrete sidewalk & curb
- No street lights & trees

Streetscape improvements have been installed in this location after the initial existing conditions assessment was performed. See Drawing 3.1 for recently installed streetscape enhancements at the intersection of Bridge Street and Hill Street.



9

Considerations:

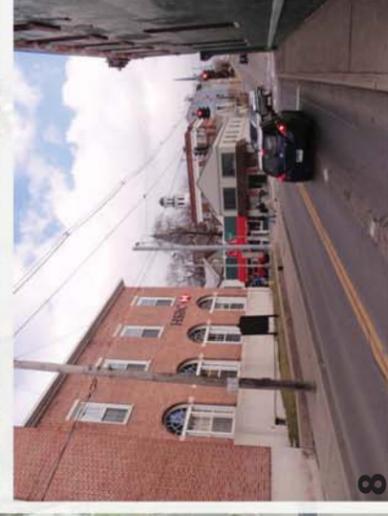
- Worn crosswalk striping
- Aging concrete sidewalk & curb
- No pedestrian crossing signal



6

Considerations:

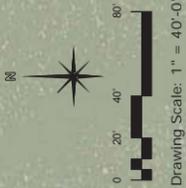
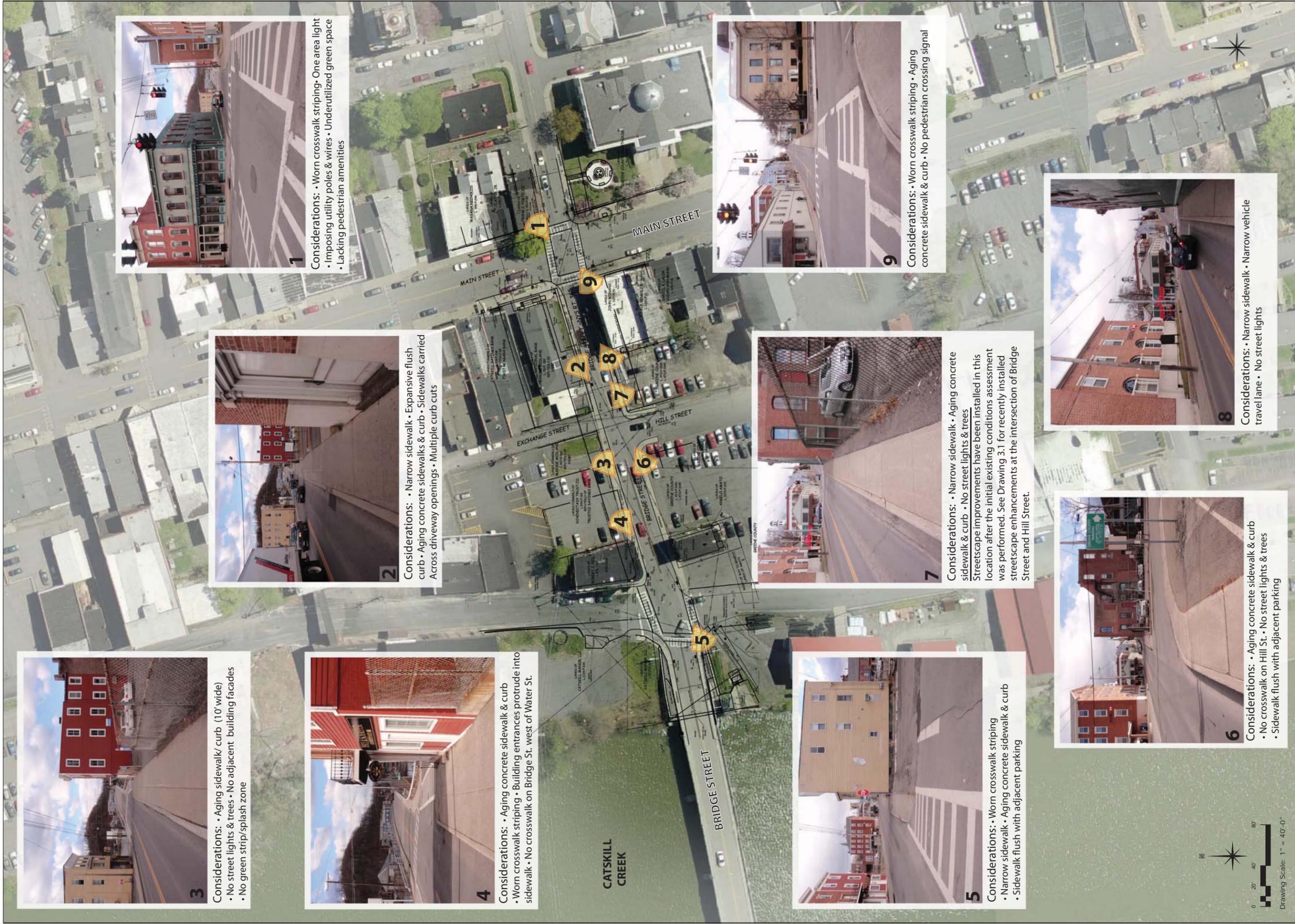
- Aging concrete sidewalk & curb
- No crosswalk on Hill St.
- No street lights & trees
- Sidewalk flush with adjacent parking



8

Considerations:

- Narrow sidewalk
- Narrow vehicle travel lane
- No street lights



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Downtown & Waterfront Enhancements
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Sheet Title:
Bridge Street Existing Conditions Photo Assessment

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Water Street

The portion of Water Street slated for enhancements spans from Bridge Street to Bronson Street. This section of Water Street is predominantly industrial in character and abuts a residential apartment neighborhood to the south. Water Street has a "worn" appearance with many of the parcels of land and buildings that cradle this corridor being vacant. Site investigation observations are listed below. Photos of existing conditions along Water Street are provided in *Drawing 1.3*.

Existing Assets:

- Close proximity to Downtown and the waterfront
- Direct access to and views of the Catskill Creek
- Key parcels identified for development
- This street has direct waterfront access



Areas for Improvement:

- Lack of sidewalks and curbs
- Worn or missing crosswalks
- Narrow road width that limits on-street parking, sidewalk widths, lighting options, and vehicular circulation
- Narrow sidewalks in locations
- Lack of street lights and trees
- Building entries crowd the sidewalks
- Steep embankment adjacent to edge of road leading to the water's edge



11
 Considerations: • No sidewalks • Necking down of road at intersection • Access drive in close proximity to intersection



1
 Considerations: • No sidewalks • No street lights • Water Street flush with adjacent parking



2
 Considerations: • No street lights & trees • Large curb cut at county parking area



10
 Considerations: • No sidewalk • No street lights & trees • No on-street parking • Guide rail at edge of the road



3
 Considerations: • Narrow/aging asphalt sidewalk • No street lights & trees • Failing retaining wall & guide rail



9
 Considerations: • No sidewalk • No street lights & trees • Narrow road • Steep embankment with guide rail



4
 Considerations: • No on-street parking • Building entries protrude into sidewalk • Narrow dilapidated asphalt sidewalk • No street lights



8
 Considerations: • No on-street parking • No sidewalk • No street lights & trees



6
 Considerations: • Fenced off vacant lot at the end of Water Street • No sidewalk • Views to the water • Connection to walking path at apartment complex



7
 Considerations: • No sidewalk • No street lights & trees



5
 Considerations: • Vacant asphalt paved lot at Bronson Street • Street flush to entire lot frontage • No street lights & trees • No sidewalk



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Sheet Title:
Water Street Existing Conditions Photo Assesment

DATE	02.16.12
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Main Street

The portion of Main Street slated for enhancements encompasses both sides of the roadway from the intersection with Bridge Street to the intersection with Bronson Street. This vehicle dominated corridor will benefit greatly from streetscape improvements. Site observations are listed below. Photos of existing conditions along Main Street are provided in *Drawing 1.4*.



Main Street pedestrian way

Existing Assets:

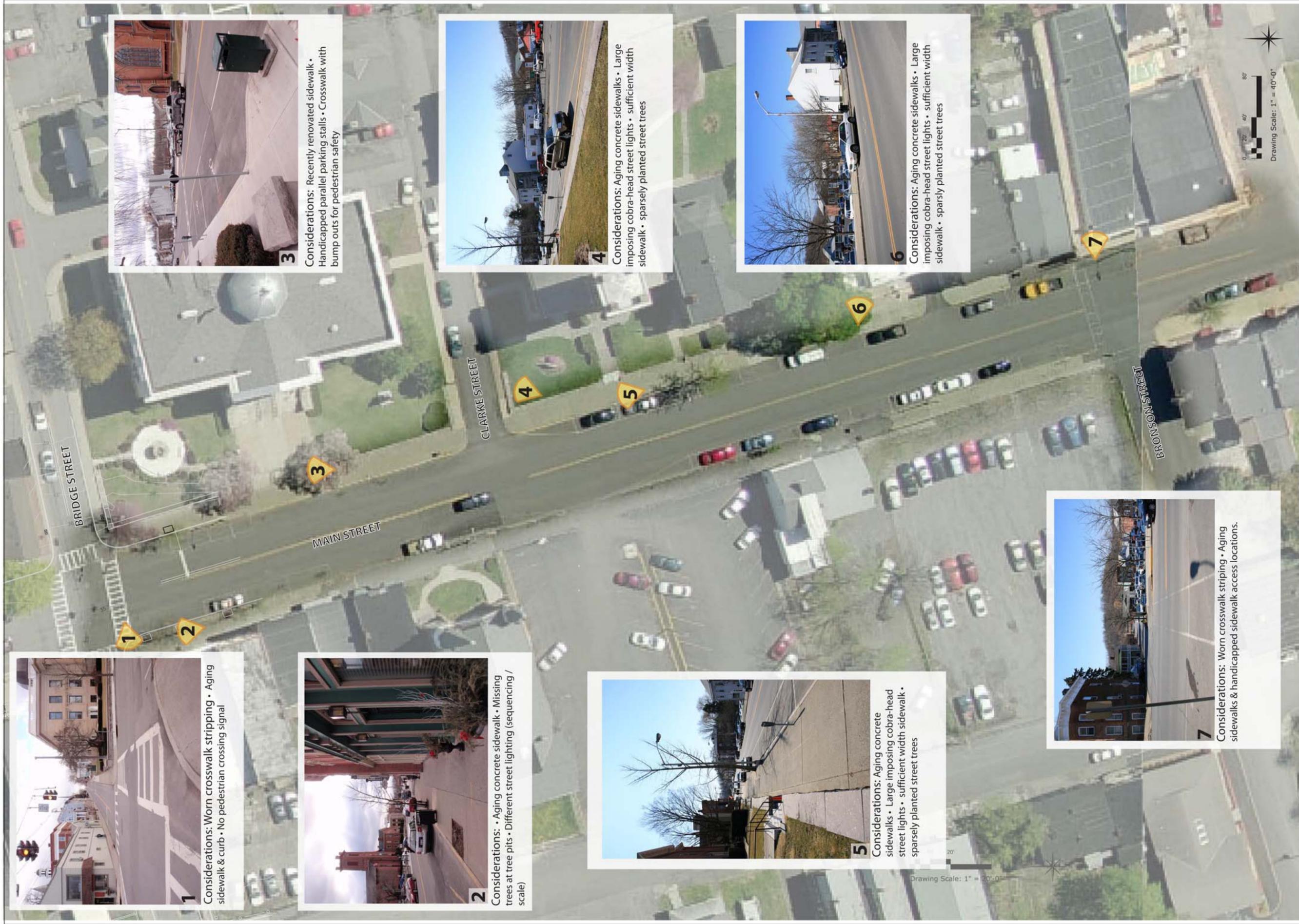
- Location at the center of downtown
- Close proximately to the waterfront
- Existing infrastructure is in place
- Recently completed streetscape improvements to the east side of Main Street at the Bridge Street intersection

Areas for Improvement:

- Ageing existing sidewalk and curbs
- Worn or lack of crosswalks in select locations
- Large scale and infrequent street lighting
- Large scale imposing traffic light pole
- Aged or missing street tree plantings



Main Street view



3
 Considerations: Recently renovated sidewalk • Handiapped parallel parking stalls • Crosswalk with bump outs for pedestrian safety



4
 Considerations: Aging concrete sidewalks • Large imposing cobra-head street lights • sufficient width sidewalk • sparsely planted street trees



6
 Considerations: Aging concrete sidewalks • Large imposing cobra-head street lights • sufficient width sidewalk • sparsely planted street trees



7
 Considerations: Worn crosswalk striping • Aging sidewalks & handicapped sidewalk access locations.



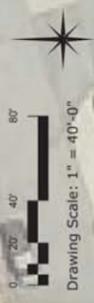
1
 Considerations: Worn crosswalk striping • Aging sidewalk & curb • No pedestrian crossing signal



2
 Considerations: • Aging concrete sidewalk • Missing trees at tree pits • Different street lighting (sequencing / scale)



5
 Considerations: Aging concrete sidewalks • Large imposing cobra-head street lights • sufficient width sidewalk • sparsely planted street trees



Drawing Scale: 1" = 40'-0"

Drawing Scale: 1" = 20'-0"

Lead Consultant:
ELAN
 PLANNING / DESIGN /
 LANDSCAPE
 ARCHITECTURE, PLLC
 18 DIVISION STREET
 STUDIO 304
 SARATOGA SPRINGS
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Sheet Title:
Main Street Existing Conditions Photo Assessment

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Enhancement Recommendations

The proposed site enhancement recommendations are intended to:

- Enhance pedestrian safety
- Provide expanded pedestrian walking opportunities
- Strengthen the physical and visual connection between the Main Street and the waterfront
- Create business and recreational activity along the Catskill creek waterfront
- Unify the downtown streetscape setting through implementation of standard lighting fixtures, street tree plantings and other street side amenities
- Develop a conceptual image of downtown waterfront in a built-out condition.

These enhancements will serve as yet another signal to interested developers of the commitment and vision that the community has in the future of the Village of Catskill. The redevelopment of Catskill's Downtown and waterfront will lie in the ability to attract private developers to assist in the redevelopment process.

❖ Catskill Trail Loop

The trail project is a “Bridge to Bridge” walking loop that starts at the parking area of the County Office Building proceeding north along Catskill Creek’s east embankment, west over the Black Bridge (currently closed), south along West Main Street, east over the Uncle Sam Bridge, and then returns north along Catskill Creek traversing two private parcels and arriving back at the County Building parking area. Owners of the private parcels have agreed in concept to provide access easements for the trail routing. See *Drawing 2.1* for trail routing and material options.

The trail will utilize various surfacing types as it traverses Village sidewalks, the creek embankment, and wooded settings. The waterfront from the Uncle Sam Bridge to the County parking area on the east shore of the Creek is designated as trail on level ground and runs along the top of the Creek embankment with minimal disturbance. A boardwalk system is recommended adjacent to the County parking area where the width is limited between the paved parking area and top edge of the embankment. North of this section is the trail through vehicular zones. Pavement treatments and markings are recommended in this area. The trail then crosses the Black Bridge to connect to the west bank of the Creek. The Black Bridge is an old train trestle that is dilapidated and has been closed due to safety concerns. The Black Bridge is a vital part of the connection of the loop and currently funding is being pursued to renovate the Bridge and its Super Structure. Sidewalks exist along the west side of West Main Street and can be used with spot repair of the sidewalks in areas. The possibility exists for an alternative waterfront trail segment in this area on the School District parking area providing access to the public boat launch and connecting to the Uncle Sam Bridge. The existing sidewalks can then help close the loop back across the Creek. In addition to the stated trail routing, the introduction of sidewalks along Water Street from the County parking area to the intersection of Bridge Street and Water Street is recommended to improve access to the trail loop from the Downtown. See *Drawing 2.2* for trail routing and surface types.

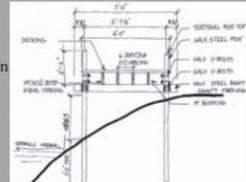
This portion of the Trail Loop where the trail crosses through the County parking area and connects to Bridge Street will utilize traffic calming techniques. Improving pedestrian and motorist safety and enhancing the visual quality of the area is vital to the revitalization and reintegration of Water Street to the Downtown and waterfront. The recommendation to employ traffic calming will reduce vehicle speeds and increase pedestrian awareness while creating a pedestrian friendly streetscape environment. Examples of traffic calming techniques recommended for this area include, but are not limited to landscaped curb bump outs and neckdowns to reduce traffic speeds and the distance of the roadway pedestrian crossing, introduction of sidewalks, narrow drive lanes, curbed and marked islands, raised and marked walkways and crosswalks, signalized pedestrian roadway crossings, posted speed limits, raised intersections, speed tables/ humps, chicanes or alternating curb extensions, road realignment, midblock neckdowns or chokers. One or a combination of all or some of these proven traffic calming techniques will be evaluated and utilized moving forward into more detailed Design Development, Construction Documentation and Construction of this project.

On *Drawing 2.2 Site Amenities Palette and Recommended Enhancements* the designation “Enhancement Nodes” has been given to key areas along the trail loop that serve as areas of respite for trail users, gateways, and pocket parks. These areas have been strategically located to increase public awareness of the great recreation opportunities in the Downtown, take advantage of available room, and provide comfortable areas of respite for trail users of all ages. Recommendations for way finding and interpretive signage, bollards, seating, fencing, lighting, and art are recommended throughout the entire length of the trail to increase interest and provide a sense of place. Public art also has a place along the waterfront. Whether the art evokes a theme of culture/ history, modern tones, environmental sensitivity, or an abstraction one or more, it can work hand in hand with an interpretive signage component to tell a story that would add to the uniqueness of the trail and help elevate local and regional awareness to promote use of the Downtown and waterfront recreational opportunities.

TRAIL ON EMBANKMENT

Located along the waterfront at the county parking lot, this section of trail is situated on a steep, erosion-prone embankment. Embankment stabilization is recommended. A traditional wood boardwalk, enhanced boardwalk with retaining wall, and a cantilevered boardwalk are options for maximizing available trail space.

Although a traditional boardwalk may be the most economical option, an enhanced boardwalk is appropriate at this location situated in close proximity to the dense use areas of the Downtown.



Traditional Boardwalk
This Low impact and low cost option provides a simple wood deck and structure for trail users to utilize the top of the embankment where space is limited.



Cantilevered Boardwalk
This high cost alternative provides increased waterfront visibility and an opportunity for lights, benches and planting enhancements.



Embankment Stabilization
The many available options of stabilization include slope nailing, geogrids, re-vegetation, etc.



Enhanced Boardwalk with Retaining Wall
This higher cost option features a decorative wall, wood decking, railings, landscaping, lighting, and benches. The robust character of the material and components in this option lends itself to the surrounding Downtown setting. Annual & semi annual upkeep of the enhanced components will be required with this option.

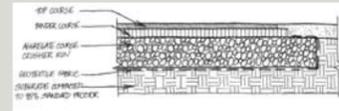
TRAIL ON LEVEL GROUND

As illustrated in the graphic below, proposed newly constructed at-grade trail sections are recommended in three locations along the Catskill Creek Trail Loop: along the east bank of Catskill Creek between the County parking area and the Uncle Sam Bridge, to the west of the Black Bridge, and in the west bank of Catskill Creek on the school district property (identified as Alternative Trail Water Trail Routing).

Traditional asphalt paving, porous asphalt paving and stone dust paving are all acceptable options for the proposed new construction of the designated Trail on Level Ground. Due to the long length of sections of the proposed at-grade trail portions, traditional asphalt paving provides the most economical, low maintenance alternative.



Alternative Waterfront Trail & Routing
Situated along the Catskill Creek at the School District parking area on West Main Street, this alternative trail routing will travel along the waterfront south to Bridge Street. Although, roadside infrastructure exists in this area the waterfront alternative is a great opportunity for trail users to reconnect with the Catskill Creek. The draw of this important environmental/recreational resource in Downtown Catskill will have a positive impact on trial use, while reemphasizing the importance that water plays in the community.



Traditional Asphalt Pavement
Smooth, durable and accessible surface that is easily maintained and repaired. Stamped and colored asphalt pavement is a decorative option of this pavement type.



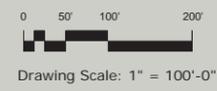
Porous Asphalt Pavement
This is a low impact/ environmentally sensitive option. This option has a higher initial material and long term maintenance costs than traditional pavement.



Stone Dust Pavement
While not as smooth as traditional and porous asphalt, stone dust provides a lower cost paving option that has an accessible surface if maintained annually.

LEGEND:

- TRAIL ON LEVEL GROUND
- TRAIL ON EMBANKMENT
- TRAIL THROUGH VEHICULAR ZONES
- TRAIL ON THE BLACK BRIDGE
- TRAIL ON NEW AND EXISTING SIDEWALK



This portion of the Catskill Creek Walking Loop will take users through the existing County parking area and lower Water Street without sidewalks. To ensure pedestrian safety, it is recommended that the pedestrian route be clearly marked. Alternatives include: painted striping, stamped/colored asphalt, and a raised pedestrian walkway with curb. Raised walkway with curb is recommended due to it being the safest and most aesthetically pleasing option. If snow removal is a concern, then stamped/colored asphalt paving is recommended in the County parking area. Painted markings should be used for crosswalks and as a pedestrian lane along the road if space is limited.



Painted Pedestrian Striping and Crosswalks
This is a low cost, low maintenance option to be used for crosswalks and pedestrian lanes when space is limited.



Stamped and Colored Asphalt
This option features a change in surface color and texture that reinforces a sense of pedestrian presence. It is a higher cost option, but is consistent with pedestrian crosswalk recommendations on Bridge Street.



Raised Walkway with Curb
This higher cost option provides increased safety for pedestrians. The curbs can be potentially problematic for snow removal. Safety is enhanced when this option is supported by bollards or landscape plantings.

The Black Bridge is a key pedestrian connector between the east and west banks of the Catskill Creek. Implementation of suspended trail surface improvements are recommended to revitalize this historic Catskill Mountain Railroad train trestle, enhance trail connectivity, and improve the pedestrian experience.



Proposed Bridge Improvements (top)
Improvements include a wider deck or multiple decks to allow for shared recreation uses, pedestrian scale lighting, new decking, and wayfinding signage.

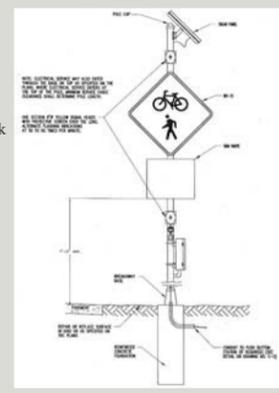


Existing Conditions (left)
The Black Bridge is characterized by a narrow walkway, peeling paint, a rusting metal bridge frame, and deteriorating deck way, superstructure, and piers.

This span of the trail includes improvements to existing infrastructure, as well as the construction of new sidewalks. Utilization of existing infrastructure is currently available with spot replacement of the sidewalk as needed. The construction of a new continuous length of concrete sidewalks and curbs is proposed to connect the Dunn Property to the existing County parking area along Water Street.



West Main Street at the Black Bridge
Recently installed crosswalks and sidewalks at the south end of the Black Bridge are in good condition. Limited room on the Creek side of West Main Street prevents use of the waterfront from the Black Bridge to the School District parking area. Existing infrastructure should be utilized for trail routing at this location.



Flashing Beacon Signal
A flashing beacon pedestrian signal at the south end of Black Bridge will improve pedestrian safety at the trail interface with West Main Street. Solar powered options are available.



Existing Concrete Sidewalks
Recommended improvements include spot repair of concrete sidewalks as needed, updated pedestrian crosswalks, and the addition of trail markers, lighting and landscaping along this section of the trail.



Existing Asphalt Sidewalks
Replace deteriorating existing asphalt sidewalks with concrete sidewalks/curbs for continuity.



Uncle Sam Bridge
Bridge enhancements may include lighting, informational/event banners, and gateway structures to highlight the Catskill Creek for motorists and pedestrians.



Alternative Waterfront Trail
The alternative waterfront trail routing provides additional waterfront access on the west bank of Catskill Creek and helps to balance the Trail Loop as a whole. The alternative routing will provide trail users access to the public boat launch at the School District parking area. The existing boat launch can be enhanced to create a waterfront pocket park that can serve as a resting/ viewing point for trail users.

TRAIL THROUGH VEHICULAR ZONE

TRAIL ON THE BLACK BRIDGE

TRAIL ON NEW AND EXISTING SIDEWALKS



Contract No.: T007022

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Project Title:
Downtown & Waterfront Enhancements
Village of Catskill
Greene County
New York

Sheet Title:
Routing and Trail Surface Recommendations

Catskill Creek Trail Loop

DATE: 11.16.12 -Rev. 01.31.13
DRAWN BY: ZB/RAB
CHECKED BY: JET
FILE: 08.008_2_Base
JOB NO.: 08.008.2

DRAWING NO.: **2.1**

SEATING (BENCHES)

As a trail loop, areas of respite will be needed. Pocket parks that serve as periodic opportunities for providing seating have been strategically located on the plan. Sufficient accessible wheelchair space at the bench pad should be considered.



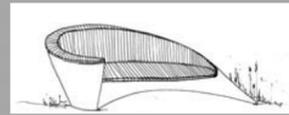
Period Style
Period style seating engages the existing historic style present in the Village's downtown.



Railroad (Historic Theme)
This bench, anchored by railcar wheels, reinforces the Village's historic tie to the Catskill Mountain Railroad.



Organic
This organic form, constructed of natural material, is symbolic of the nearby Catskill Creek.



Modern Style
This abstract suggestion of a ship's hull enforces the trails connection to the water and brings to bear a functional piece of modern art to appeal to the diverse artistic community.

AUTOMOBILE CONTROL (BOLLARDS)

Bollards are a site amenity designed to increase pedestrian safety by limiting vehicular access. Because the bollards will not be present throughout the entire trail system, an opportunity exists to select options that will enforce the trail's identity while serving as public art.



Painted Period Style (top)
Low cost artistic option that engages the existing lighting theme.

Period Style (left)
Engages the existing lighting theme at the County parking area.



Abstract Artistic Style
Whimsical function sculpture to which historic and environmental themes can be applied.

SIGNAGE

The enhancement nodes, identified on the trail graphic below, should include wayfinding and interpretive signage to enhance the user experience. Wayfinding signage provides visitors with the information needed to successfully navigate through an unfamiliar place, while interpretive signage draws attention to the site's unique history, culture and environment.



Wayfinding Signage



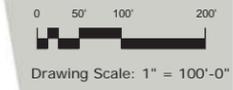
Interpretive Signage



Trail Marker

LEGEND:

- TRAIL ON LEVEL GROUND
- TRAIL ON EMBANKMENT
- TRAIL THROUGH VEHICULAR
- TRAIL ON THE BLACK BRIDGE
- TRAIL ON NEW AND EXISTING SIDEWALK
- ENHANCEMENT NODE



Fences, rails and barriers can serve as both safety measures and design features. Presented below are four design styles that build on the history or natural character of the area.



Historic Style and Materials
Wrought iron fence sections with brick pillars speak to the Village's connection to the Hudson River brick industry and are reminiscent of the regional vernacular.



Timber Barrier
This low cost option provides a non-intrusive, natural look.



Contemporary (left)
An example of historic materials such as metal and brick, presented in modern format.



Free Flowing Rail (left)
Utilization of a functional item that creates a sense of whimsy, while touching on the importance water plays in the economic success of the Village.

FENCE / RAILING/ BARRIER STYLES

Consistent lighting design throughout the length of the Catskill Creek Trail Loop will communicate a sense of project identity. Currently, there is an established period style lighting theme in the downtown and the County parking lot. The installation of trail lighting at strategic locations will make it safe and appealing to residents and tourists during evening hours.



Period Style
Utilizes existing lighting theme.



Period Style with Mounting Arm
Recommended style is similar to Village street light with slight modification to the mounting arm to signify you are on the trail.

LIGHTING

Public art contributes to the character and identity of a place. It can be clever, iconic or inspiring. The enhancement nodes, identified on the graphic above, are ideal areas to introduce public art, whimsy and a sense of place to the Catskill Creek Trail Loop.



Simple structure for local artist to display their work.



Organic/water theme can be expressed as decorative pavement.



Sculptural element with water theme.

PUBLIC ARTWORK OPTIONS



Historic railroad carving or mural.



Abstract historic railroad theme – track lengths.



Historic theme/water theme – steam wheel.



Painted historic material allows functional amenities, such as paving or walls, to express tone of artistic interest.

Lead Consultant:
ELAN
PLANNING / DESIGN /
LANDSCAPE
ARCHITECTURE, PLLC
18 DIVISION STREET
STUDIO 304
SARATOGA SPRINGS
NEW YORK 12866

Project Sponsors:

Contract No.: T007022

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Project Title:
Downtown & Waterfront Enhancements
Village of Catskill
Greene County
New York

Sheet Title:
Site Amenities Palette & Recommended Enhancements

Catskill Creek Trail Loop

DATE	11.16.12 -Rev. 01.31.13
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FILE:	08.008_2_Base
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DRAWING NO.:
2.2

❖ Bridge Street Streetscape Enhancements

The Bridge Street project consists of streetscape enhancements that include street lights, trees, new sidewalk, and curb replacement. Bridge Street is an important link connecting the downtown business district of Main Street to the Catskill Creek waterfront and the potential infill redevelopment along Water Street. Proposed enhancements are drawn from the recommendations discussed on *Drawing 2.1, Catskill Creek Trail Loop Routing and Trail Surface Recommendations* and *Drawing 2.2, Catskill Creek Trail Loop Site Amenities Palette & Recommended Enhancements*. As the main link between the waterfront and downtown and with the bulk of its infrastructure in place, survey information acquired, and minimal potential change to the streetscape at this location anticipated, the Bridge Street project area has progressed the furthest of the four project areas. The information completed at this location represents design development level work and is ready for construction documentation, permitting and construction as soon as funding is available.

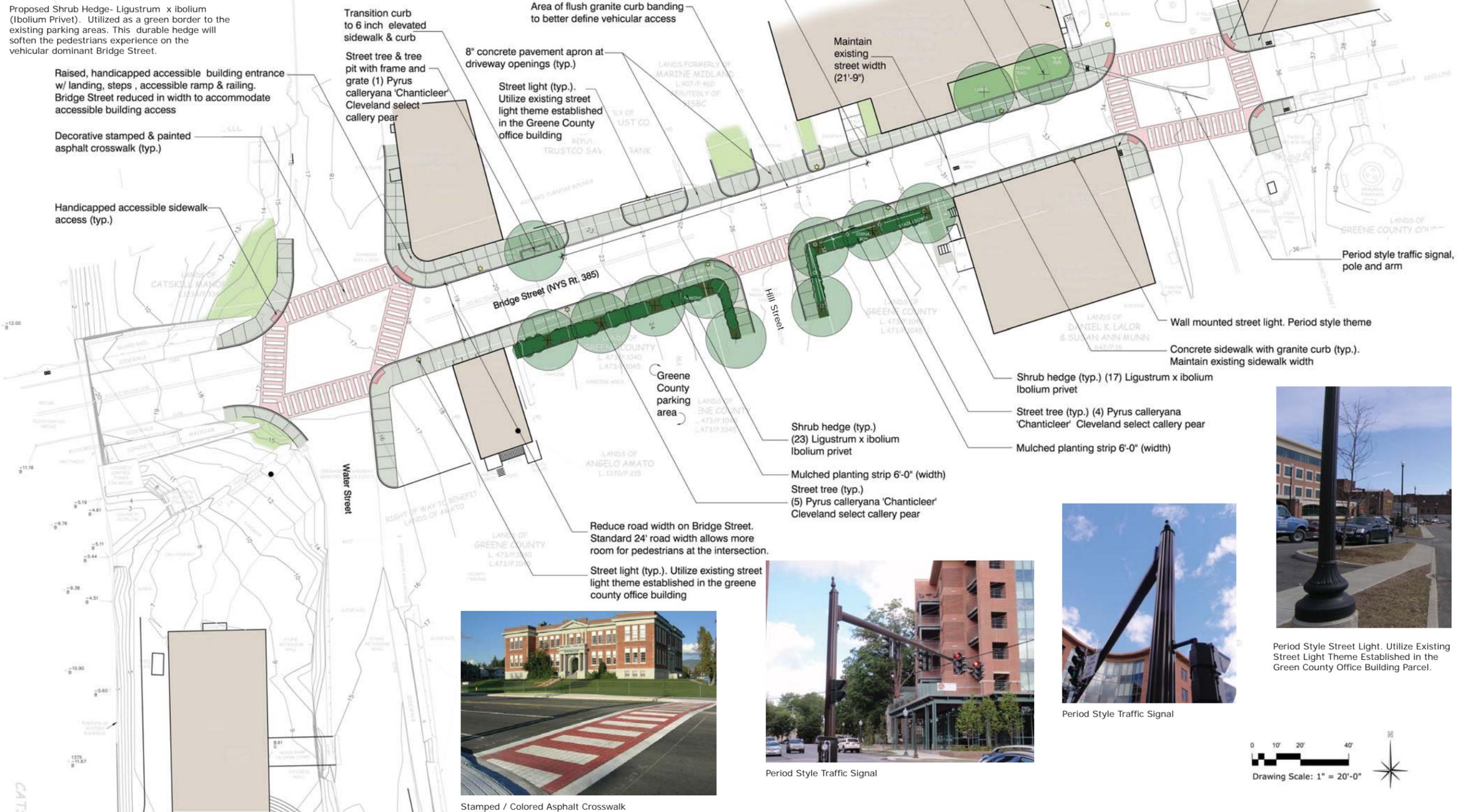
To encourage pedestrian travel between the Downtown and the waterfront, the following enhancements are recommended for the Bridge Street corridor: installation of imprinted and colored asphalt crosswalks at intersections to reinforce the pedestrian presence, installation of handicap accessible sidewalks, and replacement of the current cable/wire hanger system light pole at the northwest corner of Bridge and Main Street with a new period style traffic pole which will reduce visual clutter, and support the established period style lighting theme. It is also recommended that the existing sidewalks and curbs be replaced with new concrete sidewalks and granite curbs to rejuvenate the visual quality of the area, and that street trees be installed along the corridor to increase traffic calming and soften the harshness of the predominantly open area dominated by hard surfaces such as concrete and asphalt. The majority of the plantings have been recommended on the south side of Bridge Street near the Hill Street intersection. Due to numerous curb cuts and large expanses of flush curb and sidewalks, street trees have only been recommended in three locations along the north side of the street. It is recommended that a crosswalk and handicap accessible sidewalk access be provided across Hill Street. At the southern terminus of the Water Street intersection the narrowing of the road to a standard twenty four foot roadway width is recommended to provide more pedestrian space on the sidewalks and allow for the introduction of a handicapped accessible entrance for the building on the northeast corner of the intersection. See *Drawing 3.1* for recommended improvements along Bridge Street.



Proposed Shrub Hedge- Ligustrum x ibolium (Ibolium Privet). Utilized as a green border to the existing parking areas. This durable hedge will soften the pedestrians experience on the vehicular dominant Bridge Street.



Street Tree - Pyrus calleryana 'Chanticleer' (Cleveland Select Callery Pear). Recommended for its combination of resistance to blight and limb breakage, narrow pyramidal form and flowering interest.



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Project Title:
Downtown & Waterfront Enhancements
 Village of Catskill
 Greene County
 New York

Sheet Title:
Streetscape Enhancement Plan

Bridge Street Enhancements

DATE	11.16.12 -Rev. 01.31.13
DRAWN BY:	ZB/RAB
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DRAWING NO.:	

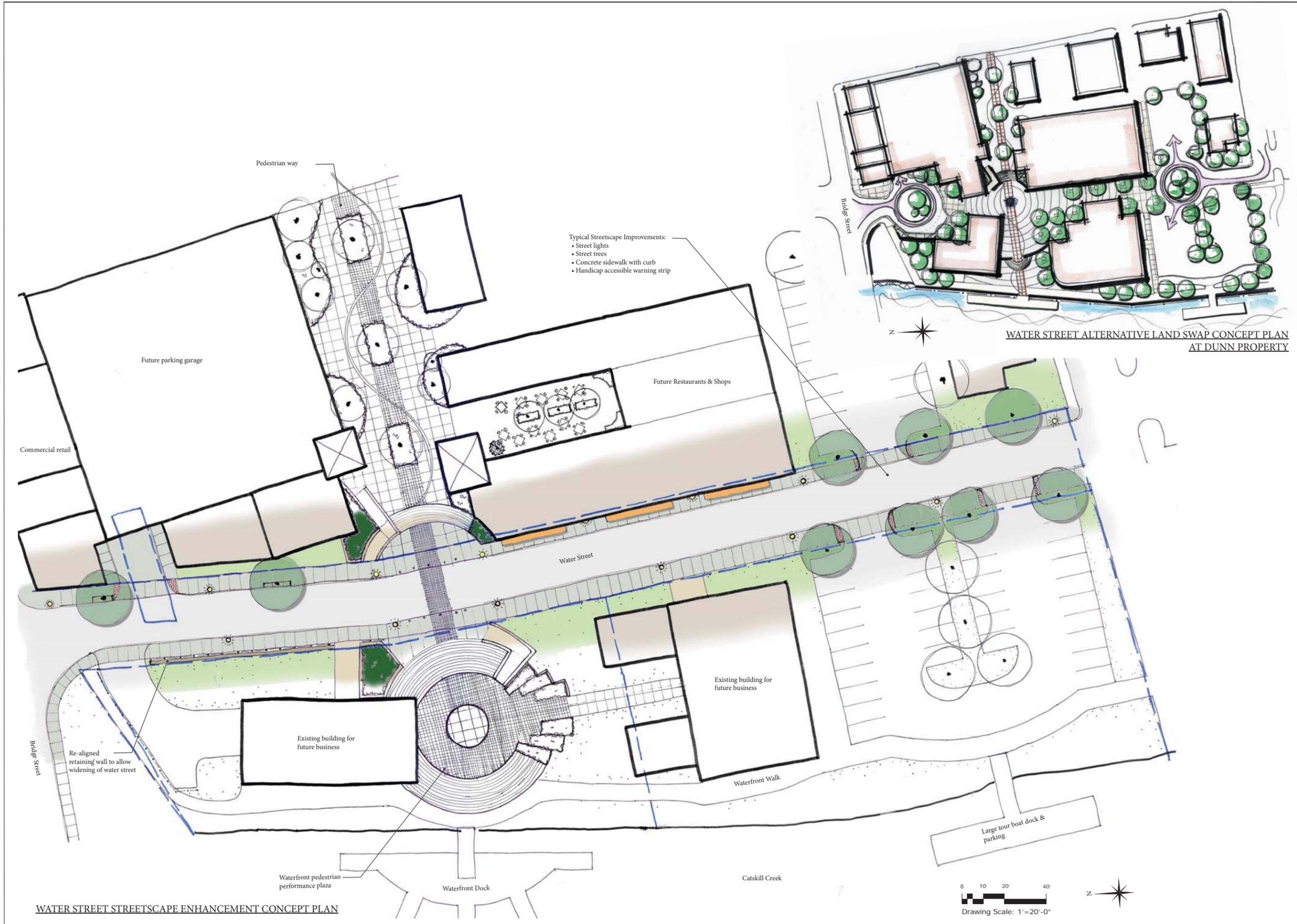
❖ Water Street Streetscape Enhancements

Predominantly industrial and commercial in character, the Water Street project area presents a number of opportunities for redevelopment of the waterfront and Downtown. The Dunn Parcel which borders Water Street along its western edge is viewed as a keystone property to revitalizing the Village. Large scale redevelopment of the vacant lots and buildings along both sides of Water Street is envisioned. The project team prepared two different development concept plans that address various approaches to vehicle circulation, as illustrated in *Drawing 3.1 and 4.1*.

The *Water Street Streetscape Enhancement Concept in Drawing 3.1* recommends many of the same streetscape improvements proposed for the Bridge Street Corridor, including the sidewalk replacement and the introduction of period style street lighting. This Plan also recommends widening the road, the introduction of a site retaining wall near the intersection at Bridge Street, and the creation of a pedestrian crosswalk that would connect Water Street to Main Street.

The *Water Street Alternative Land Swap Concept in Drawing 4.1* at the Dunn Property depicts the removal of Water Street as a vehicle way near the center of the project area. Vehicular and fire access to the interior of the site would remain at each end of Water Street. Through Village and County involvement, the roadway could be eliminated and the land could be transferred to, or purchased by, a potential developer that may not want to be restricted to the preexisting parcel sizes.

The recommended streetscape enhancements illustrated in *Drawing 3.1 and 4.1* represent design development level work. Although these plans utilized survey data, further design development and construction documentation is recommended based on the potential for the redevelopment of the surrounding area. It may be prudent to coordinate future improvements, with potential future developers at a time of their potential redevelopment projects.



WATER STREET STREETScape ENHANCEMENT CONCEPT PLAN

WATER STREET ALTERNATIVE LAND SWAP CONCEPT PLAN AT DUNN PROPERTY

Lead Consultant:
ELAN
 PLANNING / DESIGN /
 LANDSCAPE
 ARCHITECTURE, PLLC
 18 DIVISION STREET
 STUDIO 304
 SARATOGA SPRINGS
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Project Title:
Downtown & Waterfront Enhancements
 Village of Catskill
 Greene County
 New York

Sheet Title:
Streetscape & Land Swap Alternative Enhancements

Water Street Enhancements

DATE	11.16.12 -Rev. 01.31.13
DRAWN BY:	ZB/RAB
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FILE:	08.008_2_Base
JOB NO.:	08.008.2
DRAWING NO.:	

4.1

❖ Main Street Streetscape Enhancements

The Main Street Streetscape Enhancement area is located between Bridge and Bronson Streets. This section of Main Street has undergone recent improvements including sidewalk replacement and the addition of handicap accessible parallel parking stalls in front of the Greene County Court House. The Court House has added improvements with a new entrance and pedestrian area.

The Main Street project area connects directly to Bridge Street, and therefore, it is recommended that many of the same materials and themes be used to unify the two areas. Currently, Main Street has large, out of scale and infrequently spaced cobra head street lights. It is recommended that these lights be removed and replaced with pedestrian scale period style lighting which is more in keeping with the scale and charm of the Village. It is also recommended that existing concrete paved sidewalks and granite curbs be replaced and that crosswalks be handicap accessible and enhanced with imprinted and colored asphalt to further delineate pedestrian routing. Finally, it is recommended that new tree plantings be located along the western side of the roadway. Taking the cue from the existing Village fabric, the placement of street trees should be located along the road curbs in front of buildings and at the back edge of the sidewalk in planting strips at parking areas.

The recommended streetscape enhancements illustrated in *Drawing 5.1* is conceptual and the recommendations will be further refined in more detailed design via construction documents when funding is available.



Lead Consultant:

ELAN
 PLANNING / DESIGN /
 LANDSCAPE
 ARCHITECTURE, PLLC
 18 DIVISION STREET
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Project Title:

Downtown & Waterfront Enhancements

Village of Catskill
 Greene County
 New York

Sheet Title:

Streetscape Enhancement Plan

Main Street Enhancements

DATE: 11.16.12 -Rev. 01.31.13

DRAWN BY: ZB/RAB

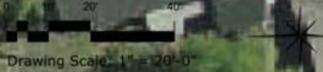
CHECKED BY: JET

FILE: 08.008_2_Base

JOB NO.: 08.008.2

DRAWING NO.:

5.1



Summary

Design Recommendations

The overarching goal of this project is to realize a revitalized waterfront and provide enhanced pedestrian connections to Downtown, while establishing a unified setting for the Village. Specific design recommendations have been broken out into the following project areas:

- Catskill Creek Trail Loop, which enhances the pedestrian walking experience by proposing a walking loop complete with “enhancement nodes” that include areas of respite for trail users, gateways & pocket parks.
- Bridge Street Streetscape enhancements help to reinforce the important link connecting the downtown business district of Main Street to the Catskill Creek Waterfront. Proposed traditional streetscape enhancements include imprinted and colored asphalt crosswalks, replacement sidewalks, street planting and period style street lights.
- Water Street Streetscape enhancements include traditional streetscape enhancements as well as proposing the creation of a future mid-block pedestrian connection from Water Street to Main Street. The removal of Water Street to promote private development in this area is a recommended alternative concept for this project area.
- Main Street Streetscape enhancements include continuation of enhancements already completed in front of the Courthouse. These enhancements include replacement of sidewalks and curbing, period style street lights, street tree planting and imprinted and colored asphalt crosswalks to delineate pedestrian routing.

These enhancements will serve as yet another signal to interested developers of the commitment and vision that the community has in the future of the Village of Catskill. The redevelopment of Catskill’s Downtown and waterfront will lie in the ability to attract private developers to assist in the redevelopment process.

Next Steps

Recently a Consolidated Funding Application (CFA) grant was awarded to Greene County to finalize design, prepare construction documents, and construct a portion of the Trail Loop project area identified in this report. This forthcoming CFA funded project will focus on the portion of the Trail along the waterfront at the County parking lot, a pedestrian connection through the parking area to lower Water Street at the “County stair case”, and streetscape enhancements to Water Street that provide the vital link between Bridge Street and the Trail Loop. The following next steps will be completed for the upcoming CFA/ Streetscape Enhancement project for Water Street and the Trail Loop:

1. **Design Development:** A site survey, specific material selection, and design improvement refinement are items that will build on the concepts and material pallet set forth in the Trail Loop portion of this *Downtown and Waterfront Enhancements Summary* report.
2. **Construction Documents and Specifications:** Detailed construction drawings and a project manual with technical specifications will be prepared for public bidding of the project.
3. **Permits:** Potential permitting and approval agencies will be formally contacted and pertinent permits and approvals will be acquired prior to bidding of the project.
4. **Bid Process and Selection of Construction Contractor:** After final design and construction documents have been approved, the project will be publicly bid, bids evaluated, and contract awarded.
5. **Construction Observation:** Construction will proceed according to the construction documents.
6. **Project Completion:** All project related work will be completed and paperwork required by the construction documents and the grant will be submitted and accounted for.

Appendix

Constructability Report

Concept Phase Construction Cost Summary

Concept Phase Cost Assessments (by project)

Constructability Report

Project: Downtown & Waterfront Enhancements
 Village of Catskill, Greene County, New York

NYS DOS Contract No.: T007022
 Elan Project Number: 08-008.2
 Date: 04.24.13 Revised 09.18.13

Item	Agency	Permit	Trigger/Threshold	Description/ Applicability	Note to File	Submitted	Received	Completed
1.1	USACE (US Army Corps of Engineers)	USACE Section 404 Clean Water Act	<input type="checkbox"/> Placement of dredged or fill material into any waters of the United States, or the performance of any work or placement of any structure into or affecting any navigable water <input type="checkbox"/> See 2007 Nationwide permit for minor projects; may be covered under national permit	It is anticipated that the only project area that may require permitting is the Catskill Creek Trail Loop.	Final jurisdiction determination and permitting will need to be addressed in future more detailed design/construction phases.			
1.2	USACE (US Army Corps of Engineers)	General Permit (nationwide or Joint permit with NYSDEC)	Any person, firm, or agency (including Federal, state, and local government agencies) planning to work in navigable waters of the United States, or discharge (dump, place, deposit) dredged or fill material in waters of the United States, including wetlands, must first obtain a permit from the Corps of Engineers. Permits, licenses, variances, or similar authorization may also be required by other Federal, state and local statutes.	It is anticipated that the only project area that may require permitting is the Catskill Creek Trail Loop.	Final jurisdiction determination and permitting will need to be addressed in future more detailed design/construction phases.			
2.0	Local Municipality	Site Plan Review, Special Use Permit, or Municipal Approval. (Specific to Municipality)	See local zoning to determine if construction activity meets threshold for SPR/SUP <input type="checkbox"/> Site Plan Review <input type="checkbox"/> Special Use Permit <input type="checkbox"/> Other Approval	Local municipal review/ acceptance may be required for each project area.	Verify if municipal review is required in future more detailed design/ construction phases. Municipality to provide a letter waving review if municipal review is not required.			
3.0	NYSDEC – Department of Fish, Wildlife, and Marine Resources	Endangered Species Coordination	<input type="checkbox"/> Endangered species coordination letter is recommended for all projects	It is anticipated that the only project area that may require agency coordination is the Catskill Creek Trail Loop.	Coordination letter will require submission during future more detailed design/construction phases.			
3.1	NYSDEC	Freshwater Wetlands	Generally limited to those freshwater wetlands 12.4 acres or greater. REGULATED ACTIVITIES: <input type="checkbox"/> Filling, draining or excavating, grading, and dredging. <input type="checkbox"/> Constructing buildings, roadways, septic systems, bulkheads, dikes, dams, and docks. <input type="checkbox"/> Clear-cutting timber and other vegetation.	It is anticipated that the only project area that may require permitting is the Catskill Creek Trail Loop.	Based on current available information, freshwater wetlands are not present on any of the four project areas. Verify presence of freshwater wetlands in future more detailed design/ construction phases.			
3.2	NYSDEC	Protection of Waters	<input type="checkbox"/> Modification or disturbance of the bed or banks of protected streams that are classified C (I) and above, including removal of sand or gravel. <input type="checkbox"/> Filling or dredging in navigable waters. <input type="checkbox"/> Construction, reconstruction, or repair of certain dams. <input type="checkbox"/> Construction, reconstruction, or modification of certain docks, mooring areas or other structures in navigable waters (effective late 1994).	It is anticipated that the only project area that may require permitting is the Catskill Creek Trail Loop.	Final jurisdiction determination and permitting will need to be addressed in future more detailed design/construction phases.			
3.3	NYSDEC	SEQR	SEQR required for Type I and Unlisted Actions (list is abbreviated – see original law for specifics) <input type="checkbox"/> maintenance or repair involving no substantial changes <input type="checkbox"/> repaving of existing highways not involving the addition of new travel lanes; <input type="checkbox"/> street openings and right-of-way openings for utility repair	The current project/ design report is viewed as a Type 2 Action under and does not require additional SEQR coordination. Significance will need to be determined for each of the four individual project areas in future more detailed design and construction phases.	Final SEQR determination can not be made until future more detailed design phases. Elan to provide memo addressing current SEQR status.			
3.4	NYSDEC	State Pollutant Discharge Elimination System (Point Source)	<input type="checkbox"/> Construction or use of an outlet or point source discharging into the surface waters or groundwater of the state. <input type="checkbox"/> Increase or alteration of the content of the wastes discharged from an outlet by a change in volume, or by a change in the physical, chemical or biological characteristics of the discharge. <input type="checkbox"/> Storm water discharge associated with industrial activity, including new construction disturbing five (5) or more acres.	No point source discharge is anticipated at this phase of the design process.	Jurisdictional determination required by DEC due to project size and sensitivity of nearby creek. Verify in future more detailed design/ construction phases.			

Item	Agency	Permit	Trigger/Threshold	Description/ Applicability	Note to File	Submitted	Received	Completed
3.5	NYSDEC	State Pollutant Discharge Elimination System (Construction Activity)	<input type="checkbox"/> Construction activities involving soil disturbances of one (1) or more acres; including disturbances of less than one acre that are part of a larger common plan of development or sale that will ultimately disturb one or more acres of land; excluding routine maintenance activity that is performed to maintain the original line and grade, hydraulic capacity or original purpose of a facility; <input type="checkbox"/> Construction activities involving soil disturbances of less than one (1) acre where the Department has determined that a SPDES permit is required for stormwater discharges based on the potential for contribution to a violation of a water quality standard or for significant contribution of pollutants to surface waters of the State. <input type="checkbox"/> Construction activities located in the watershed(s) identified in Appendix D that involve soil disturbances between five thousand (5000) square feet and one (1) acre of land.	The only project area that may require permitting is the Catskill Creek Trail Loop. However, as a trial type project it is anticipated that there will be minimal disturbance. If disturbance is under one acre than a permit will not be required.	Jurisdictional determination required by DEC due to project size and sensitivity of nearby creek. Verify in future more detailed design/ construction phases.			
3.6	NYSDEC	Section 401 Water Quality Certification (Section 401 or an individual certification)	<p>Individual water quality certification may be required if not covered under USACE Nationwide permit. If Army Corps of Engineers requires a Nationwide Permit than the Blanket 401 water quality Certification from DEC will be required.</p> <input type="checkbox"/> Any discharge into the Waters of the United States. However, in practice, 401 certifications are generally limited to discharges of dredged or fill material regulated under Section 404 of the Clean Water Act or construction and operation of hydroelectric or major interstate transmission facilities licensed by Federal Energy Regulatory Commission (FERC).	It is anticipated that the only project area that may require permitting is the Catskill Creek Trail Loop.	Jurisdictional determination required by DEC due to project size and sensitivity of nearby creek. Verify in future more detailed design/ construction phases.			
4.0	NYS DOT	HWP Application for Utility Work	<input type="checkbox"/> Installation, Maintenance, or repair of utilities within NYSDOT highway ROW	The Bridge Street project area is the only area that may require a permit.	A NYSDOT Utility Highway Work Permit is required in future more detailed design and construction phases if utilities are to be installed as part of the project.			
4.1	NYS DOT	HWP Application for Non-Utility Work	<p>Construction activity within NYSDOT highway ROW for non-utility projects.</p> <input type="checkbox"/> Driveway or Roadway <input type="checkbox"/> Improvement, including sidewalks, resurfacing, grading, seeding, clearing <input type="checkbox"/> Tree work <input type="checkbox"/> Miscellaneous construction, including permanent and temporary signs <input type="checkbox"/> Encroachment	The Bridge Street Project Area: Bride Street/ State Route 385 will require a NYS DOT work permit.	A NYSDOT Non-Utility Highway Work Permit Application will require submission in future more detailed design and construction phases.			
5.0	SHPO	Environmental Review	<p>Note: project review recommended for projects involving ground disturbance which hit Unlisted or Type 1 threshold under SEQR. The Lead Agency is the responsible party to initiate review</p> <input type="checkbox"/> The preliminary presence or absence of previously identified cultural resources within or adjacent to the project area, according to http://nysparks.state.ny.us/ : <input type="checkbox"/> Project site wholly or partially included within an identified archeologically sensitive area	It is anticipated that SHPO coordination will be required.	Jurisdictional determination will be required by SHPO in future more detailed design phases.			

<u>CONCEPT PHASE CONSTRUCTION COST SUMMARY</u>		Wednesday, September 18, 2013
Village of Catskill Downtown & Waterfront Enhancements NYSDOS No.: T007022		Note: (1) Costs contained herein are conceptual estimates for discussion and planning purposes. Costs will require refinement in future detailed design phases.
	PROJECT AREA	TOTAL COST
	<i>Catskill Creek Trail Loop</i>	\$655,809
	<i>Bridge Street Enhancements</i>	\$271,062
	<i>Water Street Enhancements</i>	\$355,674
	<i>Main Street Enhancements</i>	\$328,614
	CONCEPT PHASE TOTAL CONSTRUCTION COST (Including Project Development Contingency)	\$1,611,159

The total costs stated above are based on concept phase estimates completed as part of DOS Contract T007022. Consultant design and engineering services are in addition to the stated values and shall be identified at such a time that the projects are undertaken. This cost data was established respective of 2013 construction costing information. Actual construction costs may vary depending on construction schedule.

CONCEPT PHASE CONSTRUCTION COST ASSESSMENT					Tuesday, September 18, 2012	
Catskill Creek Trail Loop					Note: (1) Costs contained herein are conceptual estimates for discussion and planning purposes. Costs will require refinement in future detailed design phases.	
NYSDOS No.: T007022						
Village of Catskill Downtown & Waterfront Enhancement						
ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)	
A. Erosion & Sediment Control						
1	Temporary Silt Fence and Inlet Protection	1	LS	\$3,000.00	\$3,000	
<i>SUBTOTAL A</i>					<i>\$3,000</i>	
B. Demolition & Earthwork						
1	Asphalt and concrete demolition	1,525	LF	\$17.00	\$25,925	
2	Earthwork (8" across entire site)	350	CY	\$28.00	\$9,800	
<i>SUBTOTAL B</i>					<i>\$35,725</i>	
C. Utilities						
1	Electric Service	1,200	LF	\$12.00	\$14,400	
2	Area Lighting	18	EA	\$2,200.00	\$39,600	
<i>SUBTOTAL C</i>					<i>\$54,000</i>	
D. Paving and Curbs						
1	Asphalt pavement markings	1	LS	\$2,500.00	\$2,500	
2	Asphalt replacement along curb	400	SY	\$35.00	\$14,000	
3	Imprinted and colored Asphalt	300	SY	\$70.00	\$21,000	
4	Handicapped Sidewalk access/ detectable warning strip	12	EA	\$750.00	\$9,000	
5	Concrete Curbs (Consistant with existing curbs on Water Street and W. Main Street)	2,350	LF	\$30.00	\$70,500	
6	Concrete sidewalks and picnic plaza	14,700	SF	\$9.00	\$132,300	
<i>SUBTOTAL D</i>					<i>\$249,300</i>	
E. Landscaping and Site Amenities						
1	Topsoil	130	CY	\$30.00	\$3,900	
2	Trees	20	EA	\$450.00	\$9,000	
3	Shrubs	20	EA	\$100.00	\$2,000	
4	Benches	6	EA	\$1,500.00	\$9,000	
5	Bollards	8	EA	\$1,000.00	\$8,000	
6	Trail Marker Signs	10	EA	\$200.00	\$2,000	
7	Trail Information/ Wayfinding Signs	2	EA	\$6,000.00	\$12,000	
8	Timber Barrier	60	LF	\$40.00	\$2,400	

ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)
9	Public Art Allowance	1	LS	\$35,000.00	\$35,000
10	Boardwalk	130	LF	\$550.00	\$71,500
	<i>SUBTOTAL E</i>				<i>\$154,800</i>
<i>SUBTOTAL 1(A-E)</i>					
					<i>\$496,825</i>
	Mobilization (5% of "SUBTOTAL 1")				\$24,841
	General requirements (5% of "SUBTOTAL 1")				\$24,841
	<i>SUBTOTAL 2</i>				<i>\$546,508</i>
	20% Project Development Contingency = 20% of "SUBTOTAL 2"				\$109,302
	<i>SUBTOTAL 3</i>				<i>\$655,809</i>
	<i>CONCEPT PHASE CONSTRUCTION TOTAL</i>				<i>\$655,809</i>

<i>CONSULTANT SERVICES:</i>	
<i>The dollar amount stated is based on the NYSDOS Work Program for NYSDOS Contract C100220</i>	<i>\$135,000</i>

CONCEPT PHASE CONSTRUCTION COST ASSESSMENT Bridge Street Enhancements NYSDOS No.: T007022 Village of Catskill Downtown & Waterfront Enhancement					Wednesday, September 18, 2013 <u>Note: (1) Costs contained herein are conceptual estimates for discussion and planning purposes. Costs will require refinement in future detailed design phases.</u>	
ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)	
A. Erosion & Sediment Control						
1	Temporary Silt Fence and Inlet Protection	1	LS	\$3,000.00	\$3,000	
	<i>SUBTOTAL A</i>				<i>\$3,000</i>	
B. Demolition						
1	Asphalt and concrete demolition	950	LF	\$17.00	\$16,150	
	<i>SUBTOTAL B</i>				<i>\$16,150</i>	
C. Utilities						
1	Electric Service	650	LF	\$12.00	\$7,800	
2	Area Lighting	10	EA	\$2,200.00	\$22,000	
	<i>SUBTOTAL C</i>				<i>\$29,800</i>	
D. Paving and Curbs						
1	Asphalt pavement markings	1	LS	\$2,500.00	\$2,500	
2	Asphalt replacement along curb	210	SY	\$35.00	\$7,350	
3	Crosswalks	5	EA	\$750.00	\$3,750	
4	Imprinted and colored Asphalt (at Main St. Crosswalk)	150	SY	\$70.00	\$10,500	
5	Handicapped Sidewalk access/ detectable warning strip	10	EA	\$750.00	\$7,500	
6	Granite Curbs	940	LF	\$45.00	\$42,300	
7	Concrete sidewalks	7,500	SF	\$9.00	\$67,500	
	<i>SUBTOTAL D</i>				<i>\$141,400</i>	
E. Landscaping and Site Amenities						
1	Topsoil	110	CY	\$30.00	\$3,300	
2	Trees	12	EA	\$450.00	\$5,400	
3	Shrubs	45	EA	\$100.00	\$4,500	
4	Signs	12	EA	\$150.00	\$1,800	
	<i>SUBTOTAL E</i>				<i>\$15,000</i>	
	<i>SUBTOTAL 1</i>				<i>\$205,350</i>	

ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)
	Mobilization (5% of "SUBTOTAL 1")				\$10,268
	General requirements (5% of "SUBTOTAL 1")				\$10,268
	SUBTOTAL 2				\$225,885
	20% Project development contingency = 20% of "SUBTOTAL 2"				\$45,177
	SUBTOTAL 3				\$271,062
	CONCEPT PHASE CONSTRUCTION TOTAL				\$271,062

CONSULTANT SERVICES:	
20% Design & engineering contingency = 20% of "SUBTOTAL 3"	\$54,212

CONCEPT PHASE CONSTRUCTION COST ASSESSMENT					Wednesday, September 18, 2013	
Water Street Enhancements					Note: (1) Costs contained herein are conceptual estimates for discussion and planning purposes. Costs will require refinement in future detailed design phases.	
NYSDOS No.: T007022						
Village of Catskill Downtown & Waterfront Enhancement						
ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)	
A. Erosion & Sediment Control						
1	Temporary Silt Fence and Inlet Protection	1	LS	\$3,000.00	\$3,000	
	SUBTOTAL A				\$3,000	
B. Demolition & Earthwork						
1	Asphalt and concrete demolition	1,000	LF	\$17.00	\$17,000	
2	Earthwork at retaining wall	100	CY	\$28.00	\$2,800	
	SUBTOTAL B				\$19,800	
C. Utilities						
1	Electric Service	1,000	LF	\$12.00	\$12,000	
2	Area Lighting	14	EA	\$2,200.00	\$30,800	
	SUBTOTAL C				\$42,800	
D. Paving and Curbs						
1	Asphalt pavement markings	1	LS	\$4,000.00	\$4,000	
2	Asphalt replacement along curb	220	SY	\$35.00	\$7,700	
3	Handicapped Sidewalk access/ detectable warning strip	8	EA	\$750.00	\$6,000	
4	Granite Curbs	1,000	LF	\$45.00	\$45,000	
5	Concrete sidewalks	8,000	SF	\$9.00	\$72,000	
	SUBTOTAL D				\$134,700	
E. Landscaping and Site Amenities						
1	Topsoil	100	CY	\$30.00	\$3,000	
2	Trees	9	EA	\$450.00	\$4,050	
3	Signs	14	EA	\$150.00	\$2,100	
4	Retaining Wall	1	LS	\$60,000.00	\$60,000	
	SUBTOTAL E				\$69,150	
	SUBTOTAL 1				\$269,450	

ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)
	Mobilization (5% of "SUBTOTAL 1")				\$13,473
	General requirements (5% of "SUBTOTAL 1")				\$13,473
	<i>SUBTOTAL 2</i>				\$296,395
	20% Project development contingency = 20% of "SUBTOTAL 2"				\$59,279
	<i>SUBTOTAL 3</i>				\$355,674
	<i>CONCEPT PHASE CONSTRUCTION TOTAL</i>				\$355,674

CONSULTANT SERVICES:	
20% Design & engineering contingency = 20% of "SUBTOTAL 3"	\$71,135

CONCEPT PHASE CONSTRUCTION COST ASSESSMENT					Wednesday, September 18, 2013	
Main Street Enhancements NYSDOS No.: T007022					Note: (1) Costs contained herein are conceptual estimates for discussion and planning purposes. Costs will require refinement in future detailed design phases.	
Village of Catskill Downtown & Waterfront Enhancement						
ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)	
A. Erosion & Sediment Control						
1	Temporary Silt Fence and Inlet Protection	1	LS	\$3,000.00	\$3,000	
	<i>SUBTOTAL A</i>				<i>\$3,000</i>	
B. Demolition						
1	Asphalt and concrete demolition	1,000	LF	\$17.00	\$17,000	
	<i>SUBTOTAL B</i>				<i>\$17,000</i>	
C. Utilities						
1	Electric Service	1,000	LF	\$12.00	\$12,000	
2	Area Lighting	19	EA	\$2,200.00	\$41,800	
	<i>SUBTOTAL C</i>				<i>\$53,800</i>	
D. Paving and Curbs						
1	Asphalt pavement markings	1	LS	\$3,500.00	\$3,500	
2	Asphalt replacement along curb	220	SY	\$35.00	\$7,700	
3	Crosswalks	3	EA	\$750.00	\$2,250	
4	Handicapped Sidewalk access/ detectable warning strip	6	EA	\$750.00	\$4,500	
5	Granite Curbs	1,000	LF	\$45.00	\$45,000	
6	Concrete sidewalks	11,000	SF	\$9.00	\$99,000	
	<i>SUBTOTAL D</i>				<i>\$161,950</i>	
E. Landscaping and Site Amenities						
1	Topsoil	100	CY	\$30.00	\$3,000	
2	Trees	18	EA	\$450.00	\$8,100	
3	Signs	14	EA	\$150.00	\$2,100	
	<i>SUBTOTAL E</i>				<i>\$13,200</i>	
	<i>SUBTOTAL 1</i>				<i>\$248,950</i>	
	Mobilization (5% of "SUBTOTAL 1")				\$12,448	

ITEM NO.	ITEM DESCRIPTION	Quantity	UNIT	UNIT PRICE	Total Cost (including profit, labor and overhead applied to labor)
	General requirements (5% of "SUBTOTAL 1")				\$12,448
	<i>SUBTOTAL 2</i>				<i>\$273,845</i>
	20% Project development contingency = 20% of "SUBTOTAL 2"				\$54,769
	<i>SUBTOTAL 3</i>				<i>\$328,614</i>
	<i>CONCEPT PHASE CONSTRUCTION TOTAL</i>				<i>\$328,614</i>

<i>CONSULTANT SERVICES:</i>	
<i>20% Design & Engineering Contingency = 20% of "SUBTOTAL 3"</i>	<i>\$65,723</i>